

# Hamilton Road Corridor Plan



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# 1

## Introduction

## Project Overview

In 2009 the City of Gahanna conducted a strategic study of one of the community's primary economic centers, the Hamilton Road corridor, sensing that it contains seeds that can provide vital support for the future of the city's economy and quality of life. The slowing economic conditions at the time of the study created a unique and appropriate time for the community to explore new avenues of growth and redevelopment in this important corridor and gateway, which contains some of the city's most valued assets and strategic redevelopment opportunities.

## A Call To Action

Gahanna's growth over the past four decades did not come by accident. The city has consistently and successfully anticipated development opportunities, and positioned itself to build on these opportunities through proactive planning. Gahanna's success in planning for development opportunities has helped create new economic development opportunities, generate new revenue for the city, create and define a local sense of place and identity, and attract and retain residents and a skilled workforce.

Today, many of the development and economic conditions that helped to define the city are changing, creating a need to reexamine planning and economic development strategies. One area in need of reexamination is the Hamilton Road corridor. Creating a plan for this aging corridor grounded in current market conditions is important to the city to ensure future growth and development is guided under a common

vision which continues to foster a high-quality of life in the community, while creating new economic development opportunities. This plan was conducted to create a vision and plan that will help the corridor compete with other high-profile economic corridors in the region that are creating and promoting mixed-use walkable environments where a variety of activities—living, working and leisure activities—all occur in proximity.

## What Are The Issues

The Hamilton Road development pattern over the past forty years has been characterized by low development densities, separation of land uses, and a dependence on the automobile to access and move throughout the corridor. This development model from the past is no longer economically competitive. Gahanna will be most effective at capturing economic opportunities in the future by encouraging mixed-use developments that are walkable, connected to the greater community, and defined by a unique physical identity.

The purpose of this study was to develop a guide for the city to create and evaluate future development opportunities. Using this plan as a guide, the city can make informed decisions to guide the development of underutilized or under-performing land in the corridor in a way that creates a new cohesive development pattern. This new corridor vision includes medium to high densities, and other characteristics that depart from some of Gahanna's planning traditions, yet are still complementary to and supportive of those traditions.

## The Plan

### The Plan Generally...

- Establishes a vision for the future of the corridor
- Serves as a blueprint for creating an integrated land uses pattern
- Guides future growth and development
- Responds to local transportation improvements

### And serves as a tool to...

- Improve the competitiveness of Gahanna's primary commerce center
- Guide the opportunity to redevelop areas under single ownership
- Create new high-quality developments to support a variety of uses
- Ensure future redevelopment is sensitive to surrounding neighborhoods, and the overall quality of life in Gahanna
- Improve linkages and promote the corridor as a gateway into the city

Considering Gahanna as a whole, the study area offers unique depth of opportunity to foster high-value, economic growth, mixed-use, and walkable development.

At the time this planning process was initiated in 2010, the current state of the study area presented the city with a variety of opportunities and challenges that needed to be examined to create a plan for the future of the corridor. The following issues were taken into consideration while developing the plan for the corridor:

- Currently, the district contains a mix of uses which would help to create a desired destination, but the uses are segregated and need to be better linked.
- The corridor remains one of the best locations for growth in Gahanna.
- Due to the current density within the corridor, infill and increased density is the only option to accommodate new growth.
- Existing businesses should be retained.
- Access to the site will be enhanced by planned improvements to the Hamilton Road interchange at I-270.
- The corridor lacks a ‘brand’, and needs to be identified as a distinct destination.

## What Will The Plan Do?

In evaluating the challenges and opportunities in the corridor, and discussing the needs of the city, the planning team developed a plan to meet current and future development challenges, and assist the city in carefully evaluating and planning for

future development opportunities.

The Plan focuses on the impact of public investment and land-use policy, and coordinates future development with other public improvements and land-use activities. In essence, the Plan will serve as an organizing element for overall private and public sector investment.

## Planning Approach

The Hamilton Road Corridor Plan focuses on public investment, land-use policy, economic development, and transportation planning. It was important to the city that the process be inclusive and transparent, as well as balanced in a thorough technical analysis. A plan was developed that is both intuitive (public input and guidance) and technically informed (existing and future condition analyses). Of special importance is the diverse participation from a broad array of community stakeholders including residents, key property owners and city officials in the planning process.

## How To Use the Plan

The Hamilton Road Corridor Plan, establishes the a vision for the corridor. The Plan is intended to be used on a daily basis as public and private decisions are made concerning new development, redevelopment, capital improvements, economic incentives and other matters affecting the corridor. The following is a summary of how decisions and processes should align with the goals and strategies of the Corridor Plan.

### 1. Annual Work Programs and Budgets

Individual City departments and administrators should be cognizant of the recommendations of the Plan when preparing annual work programs and budgets.

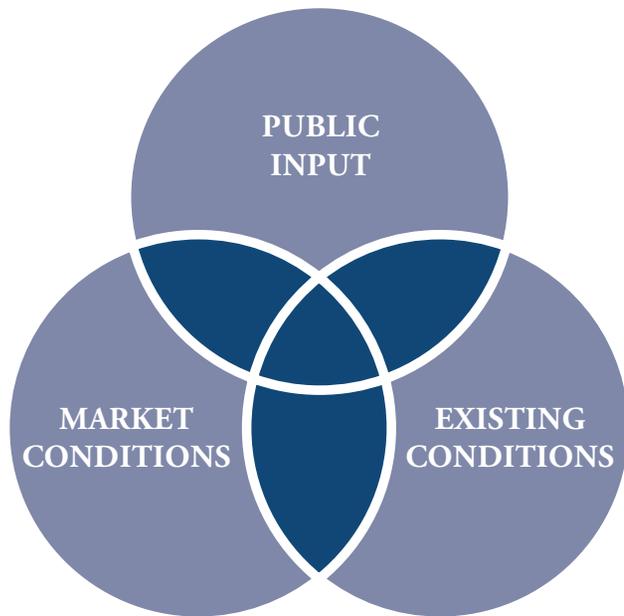
### 2. Development Approvals

Administrative and legislative approvals of development proposals, including rezoning and subdivision plats, should be a central means of implementing the Plan. Decisions by the Planning Commission and City Council should reference relevant Plan components. The Planning and Zoning Code should be updated in response to strategies presented in the Plan.



The existing condition in the planning area is shown in these two images, which represent opportunities to improve the public space along the street, and guide private redevelopment initiatives.

Figure 1: Primary Plan Drivers



### 3. Capital Improvement Program

The City's Capital Improvement Program (CIP) should be prepared consistent with the Plan's land use policies and infrastructure recommendations. New improvements that are not reflected in the Plan, which could dramatically impact the Plan's land use recommendations, should necessitate a minor update to the Plan.

### 4. Economic Incentives

Economic incentives should be reviewed to ensure consistency with the recommendations of the Plan.

### 5. Private Development Decisions

Property owners and developers should consider the goals and strategies of the Plan in their land planning and investment decisions. Public decision-makers will be using the Plan as a guide in their development

deliberations such as zoning matters and infrastructure requests.

### 6. Consistent Interpretation

City Council should collaborate with the Municipal Planning Commission to ensure clear and consistent interpretation of major items in the Plan.

### 7. Be Flexible

It is intended to serve as a guide to help the city, development community, and local residents plan for the redevelopment of the corridor. The Plan is intended to be flexible and fluid, and should be updated and amended as appropriate. As projects, policies, and programs develop over time they may not look exactly like the images in the document, but they should address the intent of the Plan. The sketches and descriptions herein provide a broad sense of how particular projects may function within the larger corridor and provide a sense of the intended development character.

## FACTORS FOR SUCCESSFUL REDEVELOPMENT

The formula for the corridor's economic success is simple: it must attract more people to live, work, and play; and it must offer them a great experience in a lively, attractive, and cohesive environment. Every corridor initiative should contribute to accomplishing this goal by:

- Drawing more regional visitors to the corridor to experience cultural and educational attractions, restaurants, businesses and other amenities.
- Attracting more people to live in the corridor— building an increased base of support for downtown businesses, restaurants, and other retail uses.
- Enhancing the corridor's identity as an appealing urban environment. The design of every individual element— from a sign to a large development or streetscape improvement—should contribute to this urban experience.
- Strengthening connections between each part of the corridor.



## Market Considerations

### Office

If The City of Gahanna establishes the Hamilton Road corridor as a newly planned and revitalized mixed-use district it will help to attract and retain new businesses over the next decade. Market demand for small multi-tenant office space could intensify in the following decade as the corridor gains recognition as a regional destination along I-270 for businesses, drawn to the area for its unique environment, brand identity, and access to I-270 and the airport. A plan that supports and accommodates freeway development is also likely to attract larger office space at the I-270 interchange as highly visible and accessible sites are becoming extinct within the central Ohio market.

### Retail

Because a substantial part of the market-demand in the coming decade involves recapturing a retail market that is currently satisfied elsewhere in the region, it is likely retail demand will grow at a substantially lower rate over the long term than what is expected in the short term, and it will be driven primarily by incremental growth of residential and office space in the corridor. Those markets will form the customer base required for new retail opportunities. Retail demand is expected to be greatest near the I-270 interchange where space is available to accommodate a large regional retailer.

Understanding how the market opportunities relate to the current economic recession was an important component of this analysis. Historical patterns suggest that periodic real estate recessions occur every 7 to 10 years. The market considerations for the Hamilton Road corridor, however, are oriented to longer-term demand trends that are expected to outlive short-term market fluctuations.

### Housing

Shifting demographic trends and preferences are generating increased de-

mand for a mix of housing types, including higher density housing recommended within the Hamilton Road corridor—housing choices that are walkable, low maintenance, and easily accessible.

Housing in the corridor should include rental housing. Renters today are increasingly “renters by choice”, creating new demand for rental housing in place of the first time home buyer, and generating support for resales in the city. Renters are more likely to remain in rental housing for a much longer time than in the past, and are more likely to delay the start of their families. More recently, renters are foregoing the typical “first time home buyer pool” by entering into the luxury rental market. The current economic climate has also improved the rental market as many home buying plans have been placed on hold. These upscale renters are excellent prospects for maintaining the resale market in the city, and the corridor is an ideal location for these housing opportunities.

Other support for this type of housing is that more and more young families on solid career paths are foregoing the typical “first-time home buyer” housing choice. Homes in the \$200,000 and over price range are increasingly the first home purchased by this group. It is therefore important that rental housing developed in the corridor be upscale and priced so they are within a step-up range that will support resales in the city.

### Market Assessment - Generation Impacts

The much discussed and anticipated change in our population brought about by Baby Boomers merging into retirement will have a dramatic impact on the housing market. The market assessment considered this demographic moving into retirement, and the migration of Gen X and Y into the housing market. These are the two largest generations in history to be impacting the housing market, and have the potential to dramatically shape how the City and corridor is developed.



Another market consideration that informed the development of the Plan is how important housing choices are to the next generation. The traditional family household as we know it is becoming a thing of the past. The next-generation of home buyers will desire a more diverse set of housing choices, price points, and amenities than those that attracted home buyers in the past. The most significant trend in the U.S. is the growing amount of the national population that is 65 years or older. Census data reports that currently 13% of the population is 65 or older; by 2030 20% will be 65 or older. This demographic will demand a new, low-maintenance housing choice that is close to amenities, and community services.

The aging demographic is creating a demand for housing that is similar to the younger generations both currently, and moving into the housing market. Now more than ever people are looking for a more dynamic lifestyle with a focus on neighborhoods that have shops, entertainment, and open space that is accessible by multiple modes of transportation.

## Hamilton Road Redevelopment Potential

The Hamilton Road Corridor combines the strength of the interchange access as a potential focus of community identity and a growing collection of locally-based shops, national chains and restaurants. In addition, a number of parcels possess the location, size, access and common ownership to offer substantial near-and longer-term redevelopment opportunities. Excellent access to regional road systems, the Columbus Metropolitan Airport, and significant natural areas including the Big Walnut Creek will contribute to the future development success of the corridor.



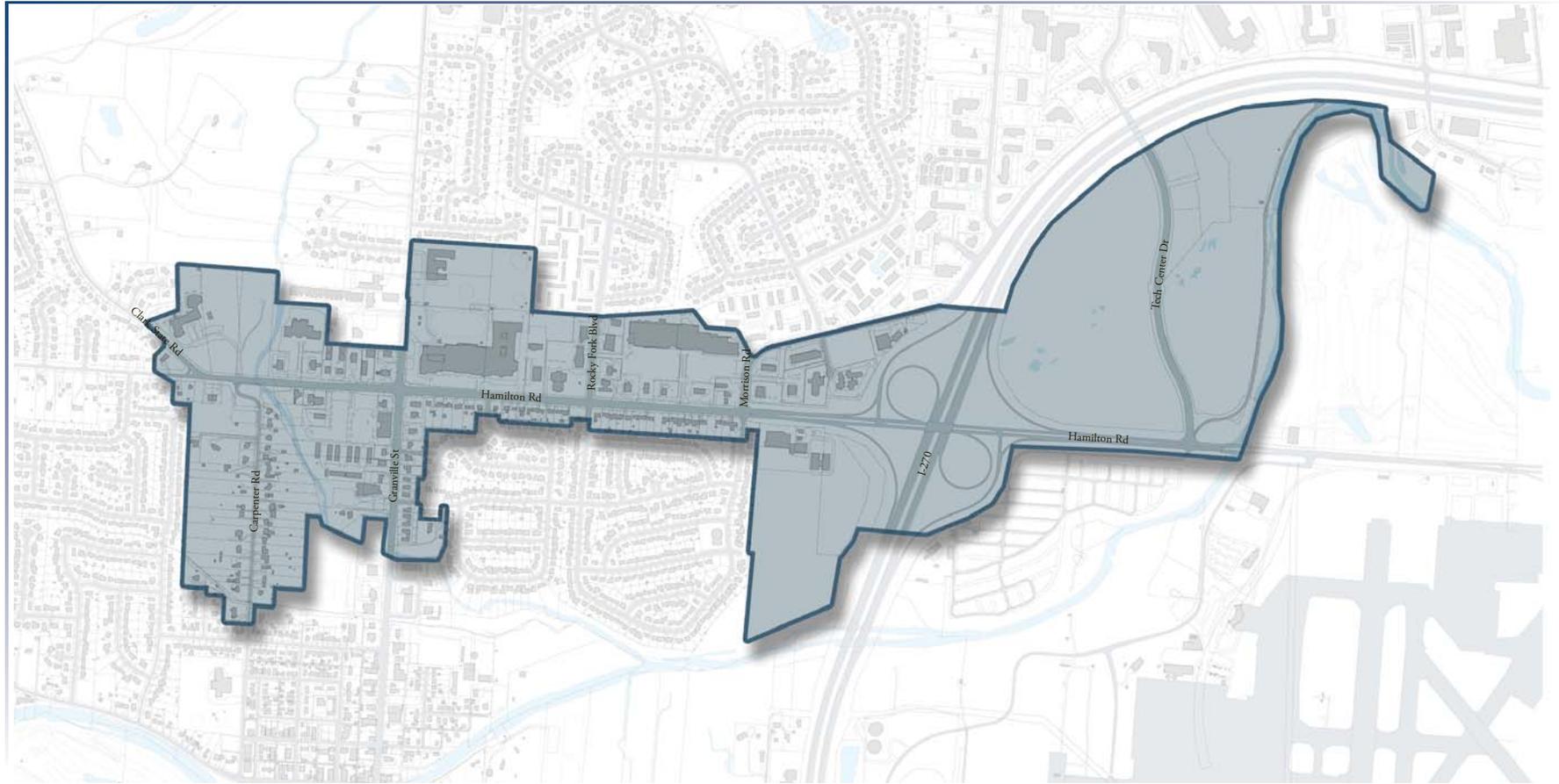
Provide a mixture of housing types within the Hamilton Road corridor to accommodate residents from first time home buyers to baby boomers. Housing types should include higher density housing that is walkable and easily accessible to the corridor.



## Planning Area Boundary

The planning area encompasses the Hamilton Road corridor and all properties that abut Hamilton Road from Clark State Road on the north end of the study area to Tech Center Drive at the south end of the study area. Other properties that encompass the study area include parcels that were contiguous to properties adjacent to Hamilton Road which were determined by the city to be of critical importance to the redevelopment success of the corridor. The planning area also included properties that abut Carpenter Road from Hamilton to Shull Avenue, as well as, properties that abut Granville Street from Gahanna Lincoln High School to the Public Library.

Map 1: Planning Area Boundary





## Key Findings & Existing Conditions



# 2

## Public Input

The process to complete the Hamilton Road Corridor Plan included extensive public input. Steering committee meetings, as well as, public open house meetings were held to get feedback from Gahanna residents and property owners about their concerns and goals for the Hamilton Road corridor. These meetings helped to gain feedback relevant to the thoughts, concerns, perceptions and suggestions that shaped the vision for the Plan.

The steering committee consisted of residents and business owners within the study area. During the meeting, participants discussed their opinions of the strengths, weaknesses, opportunities, and threats of the Hamilton Road corridor. Approximately 200 ideas were recorded through this process. The feedback gave the design team a better understanding of the internal and external factors associated with the current state of the corridor, the opportunities, and the values and aspirations of local residents.

The public open house meeting provided an opportunity to residents to give their input on the future developments within the Hamilton Road corridor. Residents gave their opinions about aesthetic improvements guidelines for the corridor. These responses influenced the recommendations for the Plan.



## Strengths

- Proximity to the airport & Net Jets Corporation
- High Traffic Flow and Visibility
- Proximity: I-270 Interchange, I-670, industrial district, downtown, Easton
- Job Growth: Central Park, Airport Joint Economic Development Strategy
- Cooperation: City, School District and Private Development Interests
- Current Mix of Uses: (residential, office, institutional, retail, etc.)
- Low Crime
- Strong school system
- Ability to build or complement existing brand if desired
- Experience developing the Creekside area
- Long stretch of road from Rickenbacker to New Albany
- Major highway: St. Rt. 317
- Large undeveloped parcel of land 120+ acres
- Off major interstate: I-270

## Weaknesses

- Lack of consistent brand/identity
- Pedestrian mobility
- Absence of quality landscaping
- No consistent theme for street trees
- No defined gateway going to or from I-270
- Many small parcel sizes and incongruent ownership
- Code enforcement issues with rental properties
- Visibility of businesses and business signs
- Traffic Capacity

## Opportunities

- Create a regional draw with a catalyst development opportunity
- Establish a brand & marketing strategy
- Prioritize redevelopment sites
- Strengthen Linkages
  - Olde Gahanna
  - Trail/ Bikeway
  - Open Space
- Showcase innovation of GLHS
- Build off proximity to airport, including public transit to/from airport
- Landscape at freeway entrance-exit
- Possible streetscape improvements on Hamilton Road, and Morrison Road to improve connection to industrial district
- Creation of a large retail park and/or tech park
- Higher education development opportunity
- Large existing vacant parcel of land adjacent to I-270 exchange
- I-270 and Granville St. create natural break points to separate land uses

## Threats

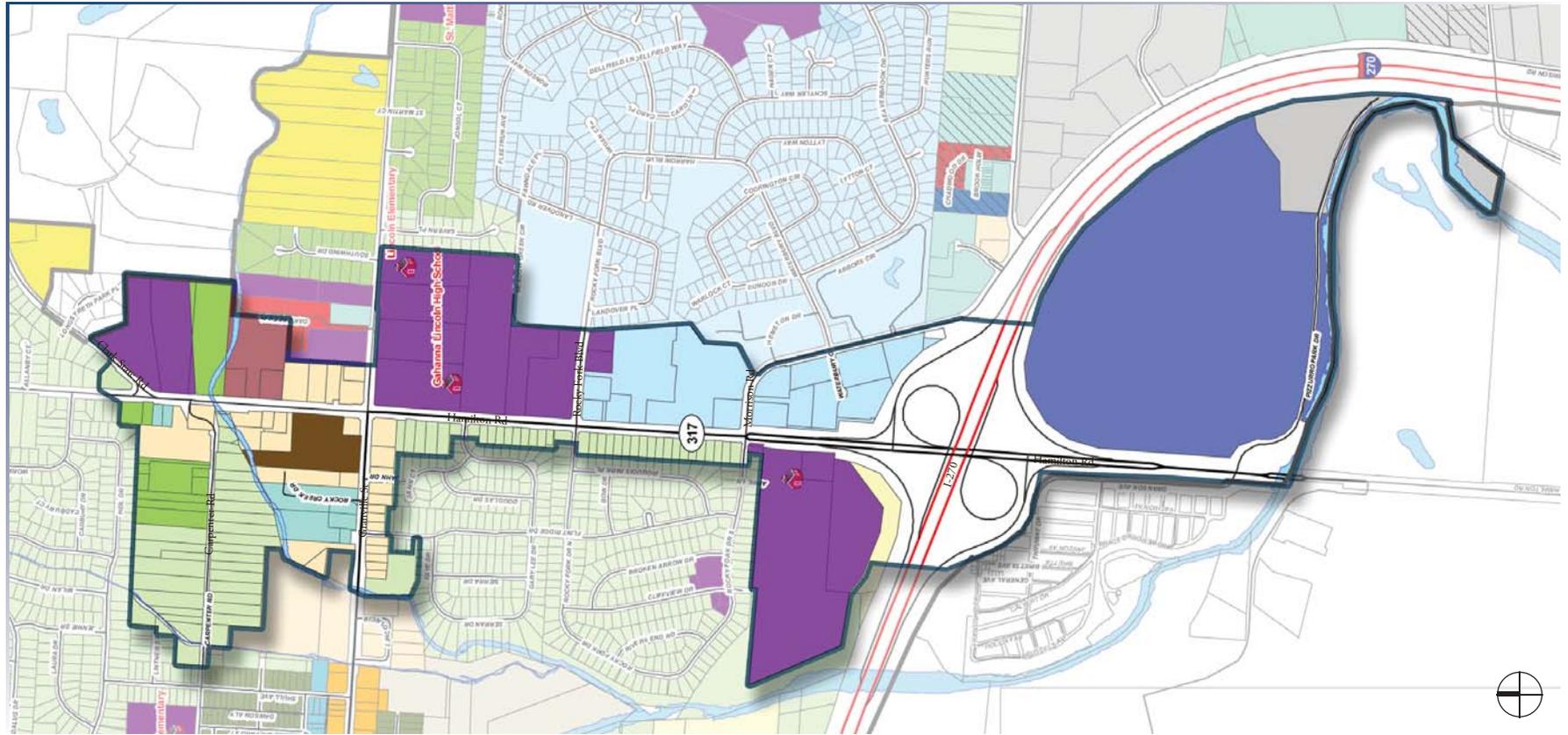
- Current economy
- Lack of consistent development standards
- Competition among similar users across multiple communities
- Apathy
- Absentee landlords
- Existing conditions (building locations in relationship to roadway, lighting, signage) are not conducive to encourage a quality retailer to locate in area
- Safety Concerns
- Unable to attract a regional draw as an anchor
- Infrastructure issues such as roads and utilities
- Homes turned into businesses



# Existing Zoning

 SF-1 Single Family Residential	 PRD Planned Residential District	 SCPD Select Commercial Planned District	 CC Community Commercial
 SF-2 Single Family Residential	 MR-1 Two Family Residential	 PCD Planned Corporate Mixed Use District	 CC-2 Community Commercial-Modified
 SF-3 Single Family Residential	 AR Multiple Family Residential	 PUD Planned Unit Development	 CX-1 Neighborhood Commercial Mixed
 R-4 Single Family Residential	 PCC Planned Commercial Center	 CS Community Service	 PID Planned Industrial Development
 ER-1 Estate Residential	 OCT Office, Commerce, and Technolog	 SO Suburban Office	 RID Restricted Institutional District
 ER-2 Estate Residential	 PRCD Planned Residential-Comm. Mixed Use District	 NC Neighborhood Commercial	 Overlay

Map 2: Existing Zoning



Hamilton Road Corridor Plan



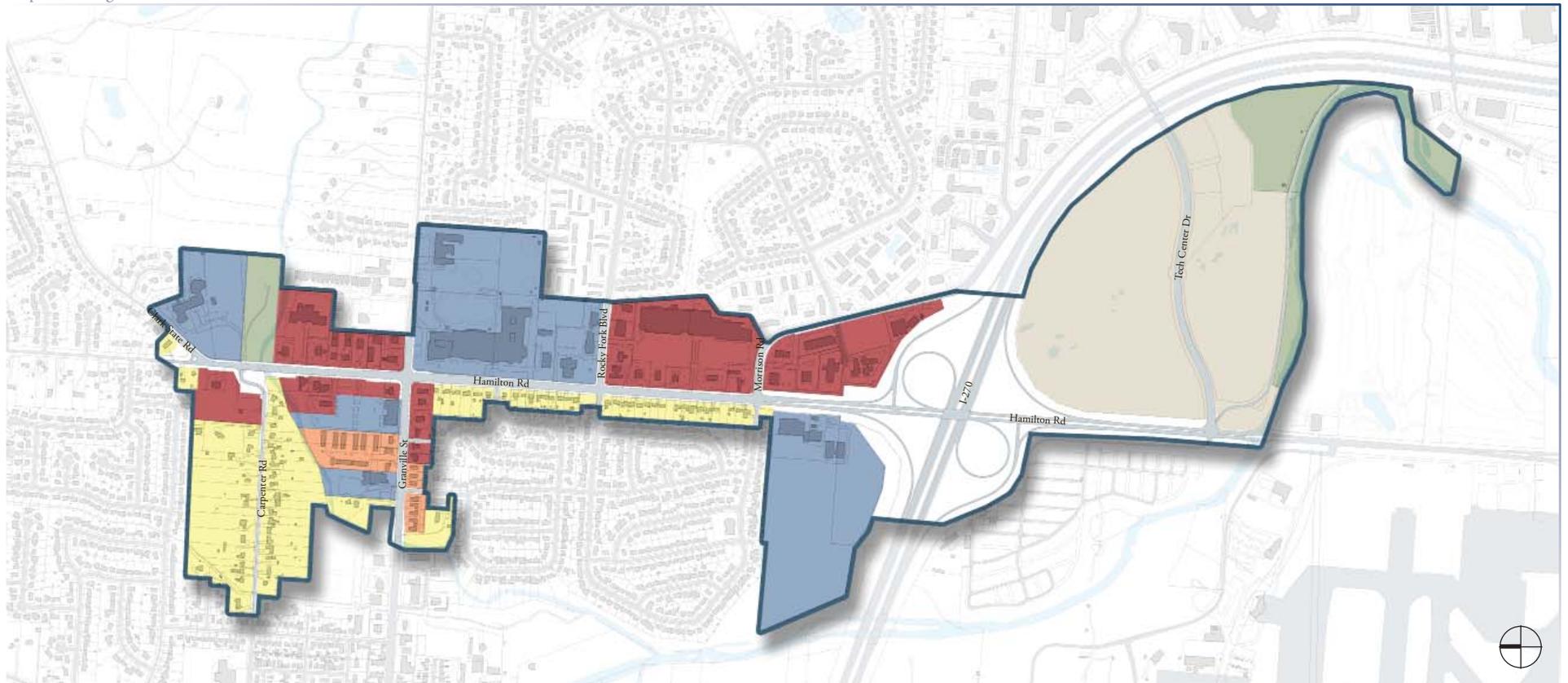
## Existing Land Uses

The Hamilton Road corridor is made up of six different land use types. These land uses are different from zoning as they describe how the land is currently being used, not necessarily how it is zoned. When evaluating the existing land uses in the corridor it is clear they are a unique mix of uses, however, the uses are segregated and minimal connections are made between the uses. This is typical for most arterial corridors developed over the last twenty-five years in the United States. When considering this configuration it is clear there are a number of opportunities to build on this framework and begin to integrate the various land use types to create a cohesive and vibrant district.

### LEGEND

	Public & Semi-Public Uses		Mixed Use
	Residential High-Density		Undeveloped Property
	Residential Low-Density		Park Space

Map 3: Existing Land Use



## Conditions Analysis

Currently, Hamilton Road contains a mixture of land uses and eclectic building styles, creating a unique corridor that provides opportunity for a wide range of development types to establish in this area. The recent redevelopment opportunities have begun to set precedents for future improvements to the Hamilton Road corridor.



- Building location encourages walkability



- Building orientation creates pedestrian friendly environment
- State of the art building programming & technology



- Architecture and building materials setting precedent
- 'Green' site initiatives



- Enhancing crosswalks to improve pedestrian safety



- Defining the corridor boundaries with landscape



- Visually enhancing the gateway entrance from I-270

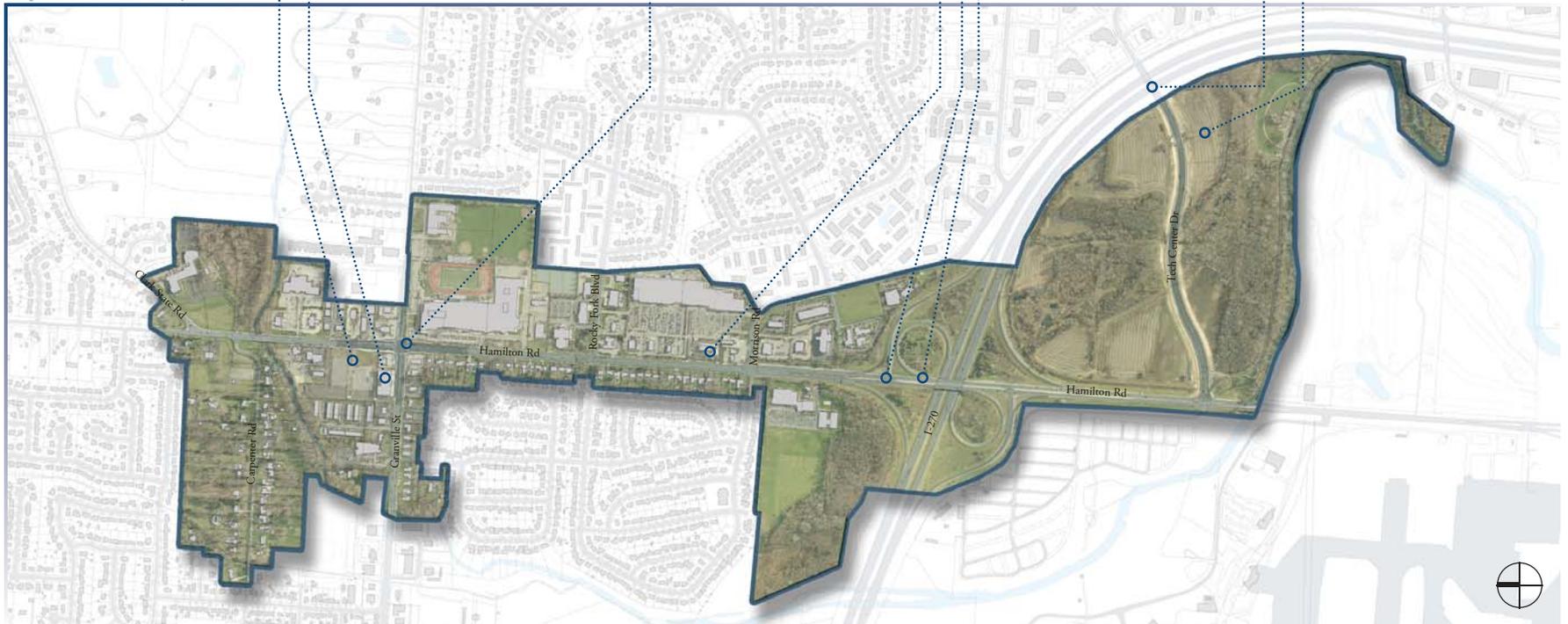


- Announcing the arrival to Gahanna with bridge signage



- Encouraging development along I-270
- Attract regional uses

Map 4: Conditions Analysis



## Planned Improvements

There were a number of planned improvements that were scheduled or underway at the time this planning study was conducted. These planned improvements were one of the primary drivers for conducting the Plan. It was necessary to consider how these improvements would support the development strategy for the corridor. The following improvements were planned or recently completed:

- Clark State Roundabout
- Clark Hall
- Commons at Clark Hall
- Granville-Hamilton Road Signal Modifications
- Relocation of McDonald's
- Huntington Bank & Tim Hortons
- Hamilton Road Raised Median Jersey Wall Improvements
- I-270 Interchange Modifications
- I-270 Bridge Improvements
- Tech Center Drive
- East Side Urologic Center of Excellence



Roundabout at Clark State intersection



Hamilton Road Gateway



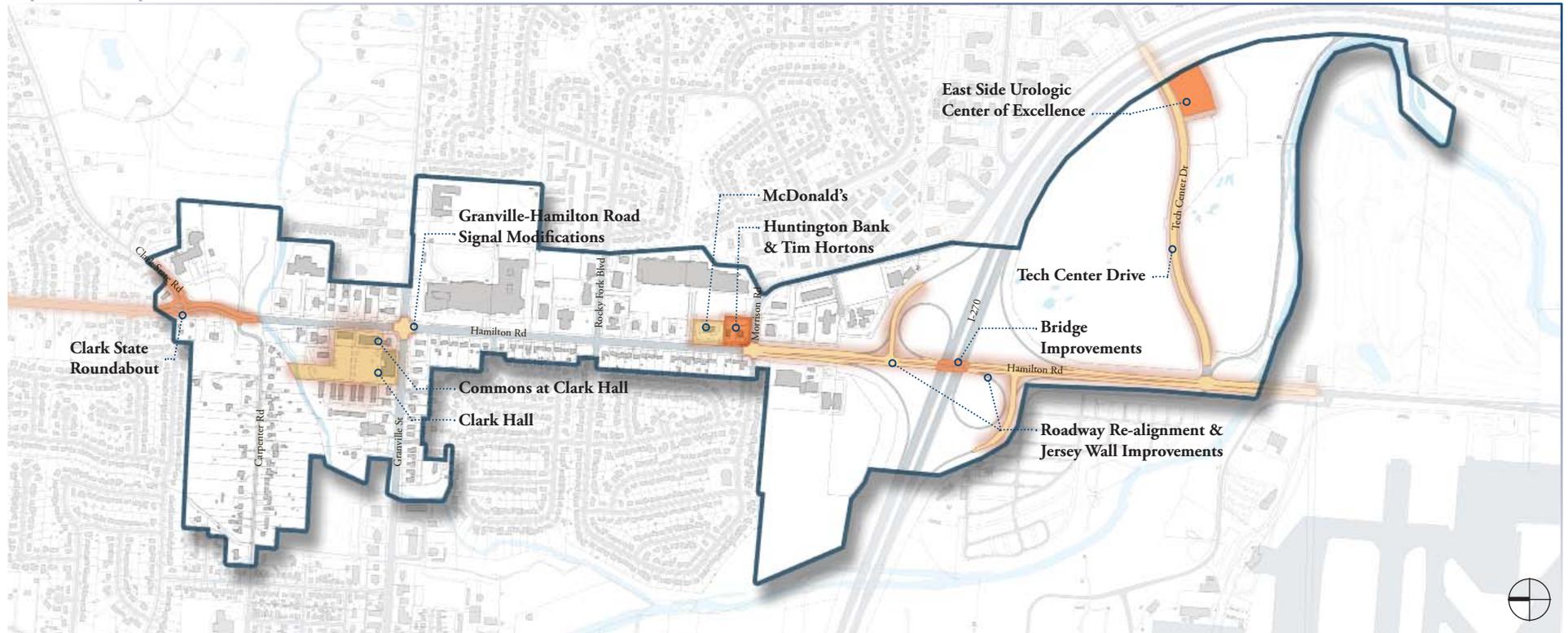
Clark Hall

### LEGEND

 Planned Projects

 Completed Projects

Map 5: Planned Improvements





# 3

## Corridor Vision

## The Plan Goals

Building on the public input received early in the planning process, the planning team and members of the Steering Committee developed several goals for the Plan. The goals, while still sufficiently broad, relate to all components of the plan and serve to focus the planning efforts on the revitalization of the corridor. The goals and objectives for the redevelopment of the corridor were created by distilling the results of the broad public input gathered during the planning process. The results were reviewed and major themes identified. These themes are illustrated in the corridor plan, and the goals and objectives of the plan.

There are seven goals for the corridor plan. Collectively these statements served as the basis for shaping the Plan, and ultimately the policies and recommendations contained herein.

### 1. Economically Diverse and Competitive:

Future development will include a mix of uses to ensure the corridor is economically competitive in the region.

### 2. Preserve What Is Gahanna:

Future development will build on Gahanna's sense of place and local traditions, while creating a distinct destination to attract and retain businesses, visitors, and residents.

### 3. Maintain and Improve Connections:

Future development will connect Gahanna's diverse community through physical connec-

tions (roads and paths), and the creation of new public spaces that serve as gathering areas.

### 4. Promote Choices:

Future development will respond to market trends and demands, specifically the aging population and growing demographic of young professionals, by providing a range of housing, entertainment, transportation, shopping and recreational opportunities.

### 5. Grow Seamlessly:

Future development will promote the corridor as a center for commerce, culture and community while safeguarding the function and success of Gahanna and adjoining neighborhoods and civic spaces.

### 6. Quality Growth:

Future development will maintain a high standard for architecture, public programming, and city services.

### 7. A Unique Brand:

Future development will integrate new urban design elements that define and brand the corridor as a unique place within the greater Gahanna community.



Gateway treatments have been created in a number of locations within the community. Similar treatments should be applied to the gateways in the study area, specifically near the I-270 interchange.



Future development in the corridor should create a cohesive and attractive streetscape that provides a welcoming and safe environment for both pedestrians and automobiles.



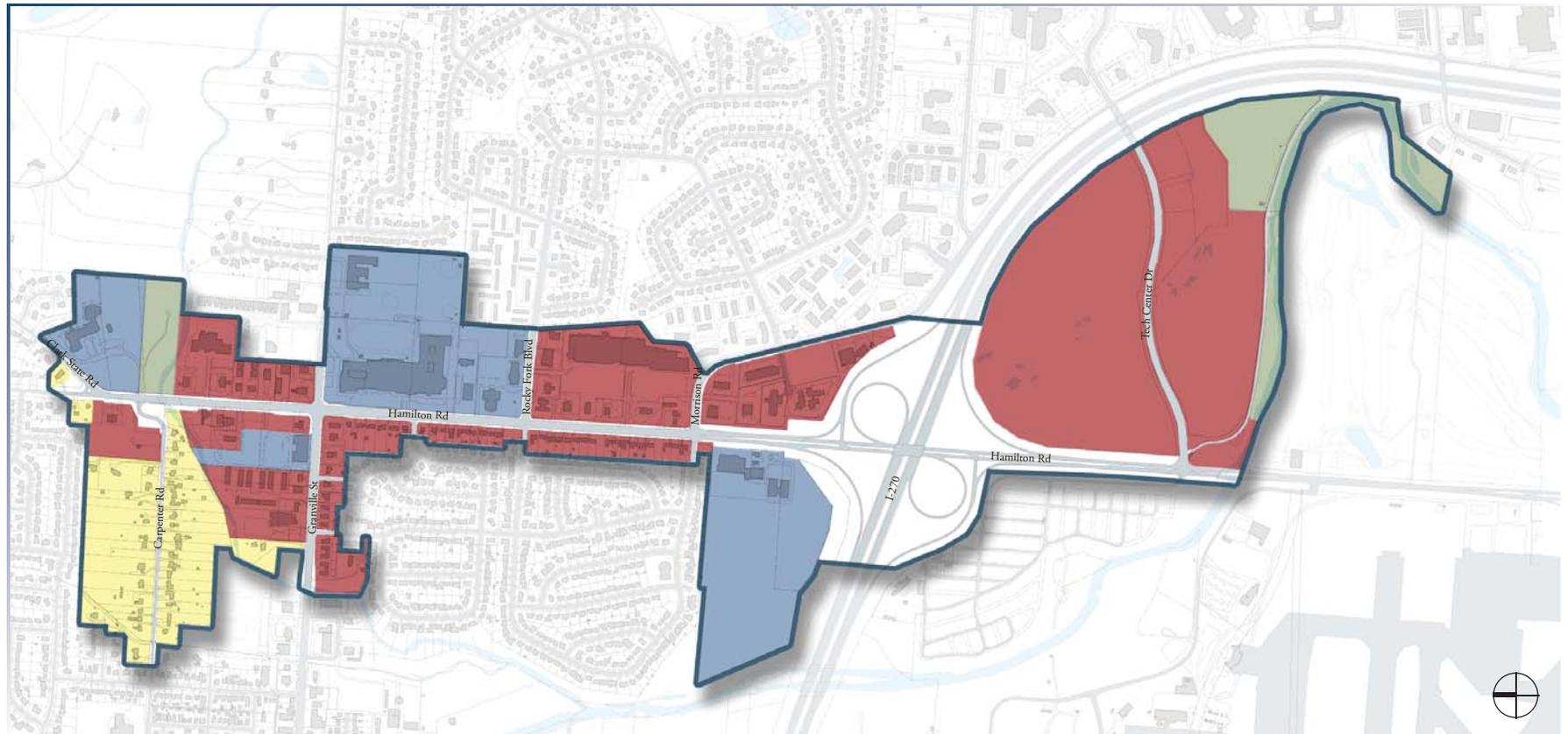
### Future Land Use Plan

The primary focus of the Hamilton Road Corridor Plan is to promote a mix of uses in the corridor and establish new connections between these uses. The plan reflects the public’s desire to expand on their unique mixed-use corridor and allow for new economic growth in line with the market potential for the corridor. The plan is intended to be flexible, while also promoting the development of a variety of housing types and future retail and office development.

#### LEGEND

- Public & Semi-Public Uses
- Mixed Use
- Residential Low-Density
- Park Space

Map 6: Land Use Plan



Hamilton Road Corridor Plan



## Corridor Districts

Looking at the corridor, it became clear there were two distinct districts defined by the transportation network and surrounding land use types and patterns. The plan was developed around these two districts, integrating land uses and transportation elements to create a unique plan for the corridor.

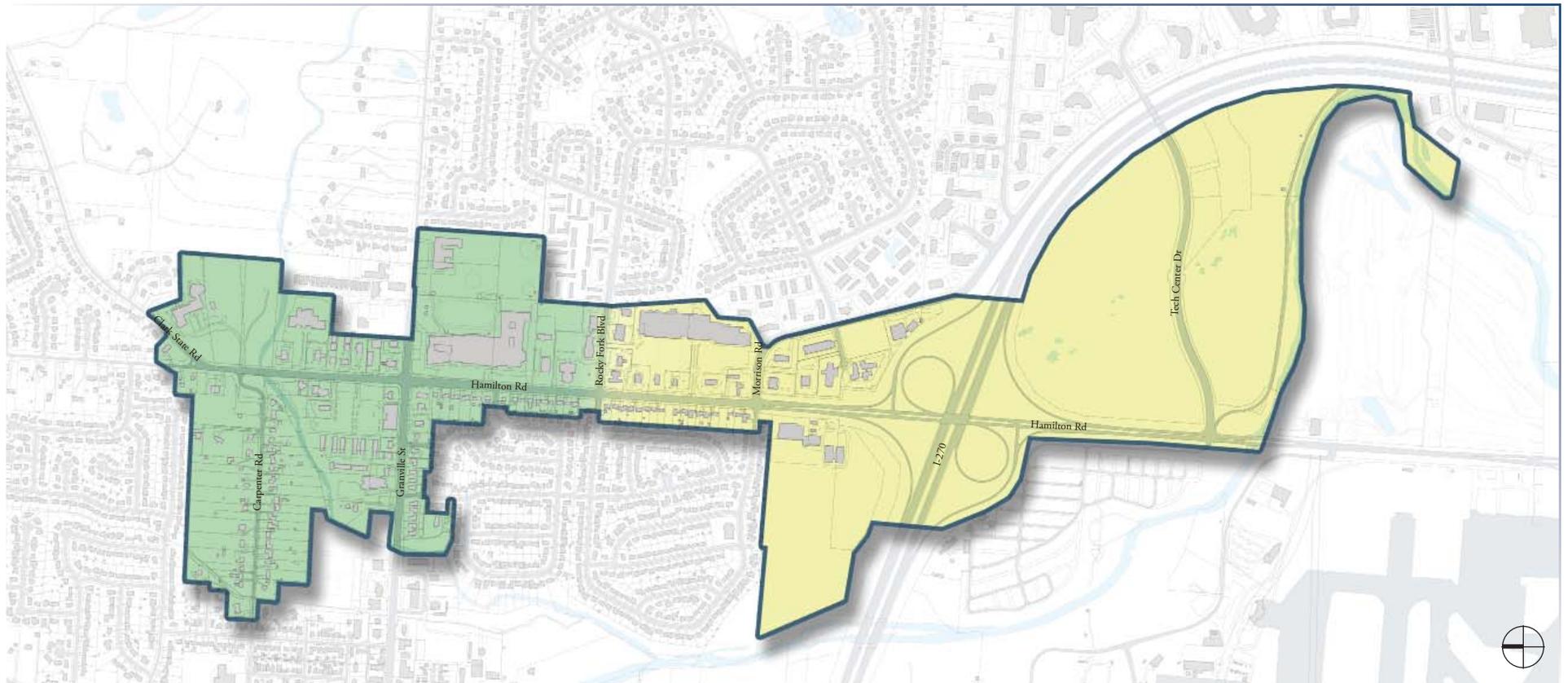
The following districts in the corridor were identified:

- *Pedestrian District*: Clark State Road to Rocky Fork Boulevard
- *Vehicular District*: Rocky Fork Boulevard to Buckles Tract Property

### LEGEND

-  Pedestrian District
-  Vehicular District

Map 7: Corridor Districts



## Franchise Architecture

Franchise architecture is commonly known as a building design that is prototypical or branded to be easily identifiable with a particular corporation and appears generic in architectural design. These standard building designs can deter a community from establishing a unique and harmonious community character.

Currently along the Hamilton Road corridor, franchise businesses occupy a large portion of the commercial buildings. Many of these businesses do not conform with the design intent of the corridor or reflect the contextual architectural character of Hamilton Road.

In order to establish the sense of place that Gahanna strives to achieve, it is necessary for the franchises to adapt their prototypical designs to comply with the architectural design intent and standards for the corridor. The architecture shall be revised to reflect the local setting and desires of the community for future developments. This will promote and reinforce the community character and brand for the City of Gahanna.

### Franchise Architecture

Franchise architecture shall complement the community character while promoting a sense of place for the Hamilton Road corridor.

Acceptable: Non-Prototype Building



Unacceptable: Prototype Building



Corridor Vision: Pedestrian District

3a



# Development Intent

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# Development Intent: Pedestrian District

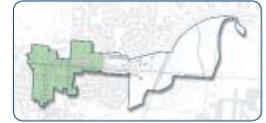


Table 1: Development Intent

Land Use Type	Development Intent	Uses (P=Preferred, O=Optional)								
		Max . Height (Stories)	SF	MF	OFF	CIV	RTL	INS	HTL	REC
<b>Public &amp; Semi-Public</b>	This area is planned for public and semi-public uses such as churches, schools, and civic buildings.	3				P		P		O
<b>Residential Low-Density</b>	This area is planned for residential uses supported by public and green spaces within or adjacent to the development. Smaller residential lots should be permitted to promote a range of housing choices and respond to changing market demand. Residential streets should include sidewalks, street trees, and connections within and outside of the development.	3	P	O						
<b>Mixed Use</b>	This area is planned for a mix of small to medium sized retail and office uses both vertically and horizontally. Office uses should be encouraged on the second floor while retail uses should be reserved for the first floor and have a strong street presence. Residential uses may be incorporated within this area. These areas should be carefully planned to complement the streetscape and help to create and define the public realm. Development should be connected via a pedestrian network and include carefully integrated public spaces.	4		O	P	O	P	O	P	O

The information presented in this table are recommendations. Design flexibility should be granted as deemed appropriate by the city. The building blocks and preferred uses may slightly vary depending on market and site conditions.

<i>SF</i>	<i>Single Family</i>	<i>RTL</i>	<i>Retail</i>
<i>MF</i>	<i>Multi-family</i>	<i>INS</i>	<i>Institutional</i>
<i>OFF</i>	<i>Office</i>	<i>HTL</i>	<i>Hotel</i>
<i>CIV</i>	<i>Civic</i>	<i>REC</i>	<i>Recreational</i>

# Aesthetic Improvement Design Intent

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## Pedestrian District: *Architecture*

### Overview

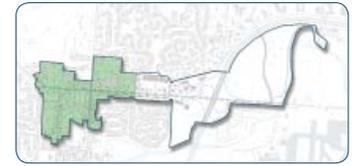
The Hamilton Road corridor is currently a mixture of architectural styles, scales, uses and building types. While many of the buildings within the corridor promote a pedestrian friendly atmosphere, the irregular nature of past development has left Hamilton Road without a truly cohesive and coherent pedestrian network. Architecture can be one of the most effective ways to convey a sense of character and pedestrian friendliness within an area. Creating architecture with a human scale through the use of materials, glazing and styles will further define and reinforce the pedestrian orientation of the district.

Input from the stakeholders and public open house identified a need for a diversity of architectural styles integrated into the corridor, with flexibility in design to encourage visual interest and add to the overall appeal of the corridor. Attendees seemed most comfortable with two story buildings within the district, and tended to dislike buildings that were perceived as too tall. Buildings that were commonly preferred all had relatively simple facades and massing, with materials and details designed to a human scale.

### Design Intent Statement

***Architecture should be high-quality and include a diversity of architectural styles to encourage visual interest and add to the overall appeal of the corridor. Building massing, shape, natural materials and traditional details should all complement pedestrian activity.***

- Aim for a “timeless design” and employ sustainable/traditional materials and design that have a proven longevity.
- The building facade should be varied to provide visual interest, while contributing to the quality and definition of the street wall.
- Buildings should be designed to the scale of the pedestrian, utilizing traditional detailing and architectural elements such as doors, windows and columns to create patterns and rhythm along the building facade.



### Likes



### Dislikes



## Pedestrian District: *Landscaping*

### Overview

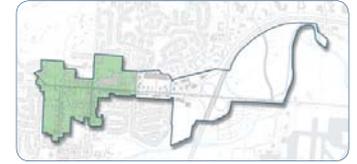
Landscaping within the pedestrian district of Hamilton Road should act to reinforce the character of the district and create a unifying image distinct from surrounding areas. In addition, it will act to define public walkways and act as a buffer between roadways, pedestrian areas and buildings.

Stakeholders and open house participants tended to like landscaping that was incorporated into a successful streetscape, whether it be in planters, planting beds, or front yard gardens. People tended to prefer a variety of plantings which added some amount of visual interest, and further defined the scale of the pedestrian area. Landscaping that seemed incidental to the site was not well liked by a majority of people.

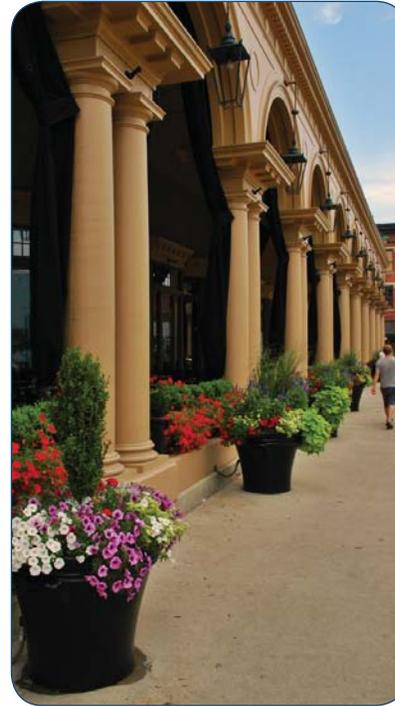
### Design Intent Statement

***Landscaping should be low maintenance, create a distinct image to the corridor, integral to the design of the site and enhance the streetscape and public realm.***

- Enhance the street and pedestrian environment through careful selection and placement of street trees and planting beds.
- Use massing to create arrangements of color and texture to soften the built environment; where applicable, species should be grouped together to create the best impact.
- Seasonal planting programs should be performed in a systematic and comprehensive way.
- Landscaping should be used to define the boundary between automobile, pedestrian and building zones, as well as provide buffering between the sidewalk and street.



### Likes



### Dislikes



## Pedestrian District: Signage

### Overview

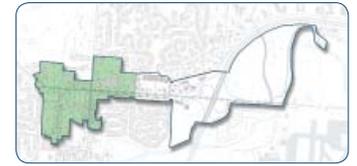
Corridor signage can help set the tone for the overall character and image of the corridor. Various types of signage should work in unison, complementing one another with similar design character but leaving enough room for creativity for each business or development to distinguish themselves from others within the corridor.

Open house participants as well as stakeholders tended to prefer signage that was simple, understated and constructed of quality materials. Preferred signage had an indirect lighting source, while disliked signage tended to be backlit or neon.

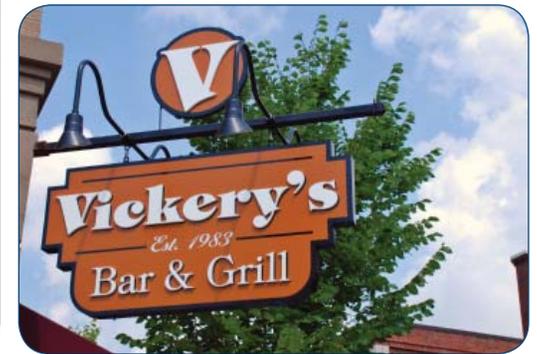
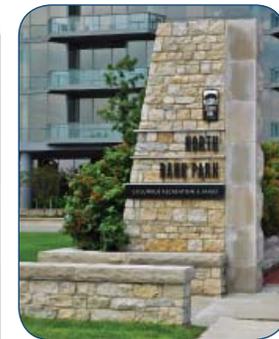
### Design Intent Statement

***Signage should be uniform, eclectic, and unique, promoting the quality and character of the corridor.***

- Signs should be conceived as an integral part of the architecture, landscape and site design so as not to appear as an afterthought. The location, size and appearance of signs should complement the building and be complementary in character with the rest of the corridor.
- Landscaping should be used to soften the base of ground signage.
- Exterior indirect lighting fixtures are the preferred source of illumination.
- Temporary sandwich board signs that add to the character of the pedestrian zone and streetscape, should be allowed.



### Likes



### Dislikes



## Pedestrian District: Streetscape

### Overview

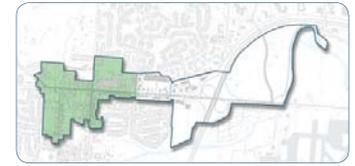
The Hamilton Road corridor can be enhanced through thoughtful, consistent streetscape design. The establishment of a defined streetscape with a well planned pedestrian zone are both critical design elements in the development of the corridor. The streetscape should provide a wide and comfortable pedestrian space, buffer the sidewalk from the street with a tree lawn or furnishings zone, and improve sidewalk appearance through paving techniques. These streetscape enhancements help to create a welcoming and desired place to travel.

Stakeholders and open house participants tended to prefer streetscapes with a clean and cohesive design, allowing a comfortable amount of space between the building and the road for pedestrian activities. All of the preferred streetscapes used street trees to divide and protect the pedestrians from automobiles.

### Design Intent Statement

***Streetscape improvements should convey a clean, simple, and consistent image that is defined by signature street trees, a unified plant palette, and identifiable landscape elements in targeted areas.***

- Streetscapes should be walkable and accommodate a variety of uses.
- Site furnishings should be provided to enliven the streetscape and provide areas for rest and interaction.
- Building setbacks should be wide enough to create a pedestrian zone 10'-20' wide from the curb in order to promote pedestrian activity as well as dining and other commercial activities.
- Street trees and landscape should be used to establish a cohesive streetscape.



### Likes



### Dislikes



## Pedestrian District: *Lighting*

### Overview

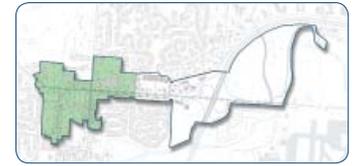
Lighting should promote pedestrian and vehicular safety and add to the character of the corridor. Good lighting has the ability to reinforce the character of a streetscape during the day, and define the character of a district during the night. In addition, lighting provides safety to both pedestrians and automobiles traveling in the district.

Open house participants and stakeholders tended to prefer traditional lighting poles and fixtures. Lighting that was generally disliked tended to have more of a modern or sculptural character.

### Design Intent Statement

***Pedestrian lighting should contribute to and reinforce the character of the district and the pedestrian scale of the street. Fixtures should be as consistent in size, style and color as possible throughout the corridor for both public and private developments.***

- Lighting should promote pedestrian and vehicular safety, and add to the character of the corridor.
- Pedestrian lights should contribute to the pedestrian scale of the street and add a warm glow of light on the sidewalk.
- The style of the pole and fixture should promote the character of the corridor. Fixtures and poles should match the same size, style and color as those used in historic downtown Gahanna.



### Likes



### Dislikes



## Pedestrian District: *Public Space*

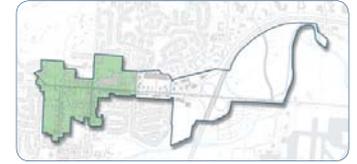
### Overview

When considering an outdoor space, the entire corridor should be taken into consideration as the public spaces will collectively help define the character of the corridor. Successful public spaces should be easily accessible by having physical connections, as well as, visibility to the space. Scale is another consideration that can both add to or detract from the success from a public space, as too big a space can leave people feeling exposed and too small a space can leave people feeling cramped. Public spaces should be well equipped with pedestrian amenities, such as ample seating, bicycle racks, waste baskets, lighting and shade.

### Design Intent Statement

***Public spaces should be open to all, well designed and promote various uses and activities. Plazas, courtyards, cafe spaces, green spaces and pocket parks are examples of public spaces that can attract people to the area and promote pedestrian activity. These spaces can be both publically or privately owned.***

- Provide public spaces in high use areas.
- Consider function, size and activity when designing a public space.
- Provide an adequate amount of seating and pedestrian amenities in public spaces.
- Integrate the use of plant material and features to enliven public spaces and promote safety.



### Likes



### Dislikes



# Pedestrian District: *Site Design*

## Overview

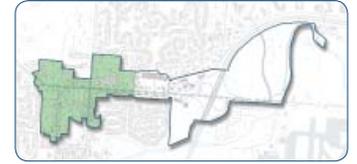
Site design refers to the relationship of elements on the site, such as the building, parking, sidewalk, green space, lighting and signage, and how all of these elements relate to the street. Good pedestrian site design will use public space to connect the building to the street. Pedestrian walkways, landscaping and site amenities are used to reinforce and define this public space. Parking, utilities and loading areas are preferably placed to the rear of the building.

Stakeholders and public meeting participants seemed to prefer buildings that were closer to the street, while allowing enough space for green space or pedestrian activity in the front. Buildings that were either too close or too far from the roadway tended to be disliked.

## Design Intent Statement

***Site elements that enhance pedestrian activity and create a feeling of place should be located closer to the street to promote and enhance pedestrian activity within the district.***

- Building setbacks should be wide enough to create a pedestrian zone 10'-20' in width from the curb to promote pedestrian activity as well as dining and other commercial activities.
- The primary building entrance and storefront windows shall face the street.
- Parking lots, loading, utilities and waste containers shall be located to the rear or side of the building. When located to the side, landscape screening shall be used to shield their visibility from the street.



## Likes



## Dislikes





# Corridor Vision: Vehicular District

3b



# Development Intent

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# Development Intent: Vehicular District

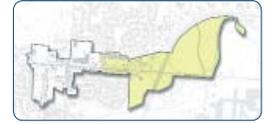


Table 2: Development Intent

Land Use Type	Development Intent	Uses (P=Preferred, O=Optional)								
		Max . Height (Stories)	SF	MF	OFF	CIV	RTL	INS	HTL	REC
<b>Public &amp; Semi-Public</b>	This area is planned for public and semi-public uses such as churches, schools, and civic buildings.	3				P		P		O
<b>Mixed Use</b>	This area should incorporate a mix of small to medium-sized, and large-scale retail and office uses. Residential uses may also be incorporated in this area. These uses intend to serve tenants from the local level to regional and national markets. This district can include highway-oriented businesses. Uses in this area should be well connected via auto and pedestrian connections. It is expected that growth in these areas should incorporate low-impact development practices, enhance the streetscape, and incorporate well-planned interior public and green spaces visible from the public realm.	7		O	P	O	P	O	P	O

The information presented in this table are recommendations. Design flexibility should be granted as deemed appropriate by the city. The building blocks and preferred uses may slightly vary depending on market and site conditions.

- |            |                      |            |                      |
|------------|----------------------|------------|----------------------|
| <i>SF</i>  | <i>Single Family</i> | <i>RTL</i> | <i>Retail</i>        |
| <i>MF</i>  | <i>Multi-family</i>  | <i>INS</i> | <i>Institutional</i> |
| <i>OFF</i> | <i>Office</i>        | <i>HTL</i> | <i>Hotel</i>         |
| <i>CIV</i> | <i>Civic</i>         | <i>REC</i> | <i>Recreational</i>  |

# Aesthetic Improvement Design Intent

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## Vehicular District: *Architecture*

### Overview

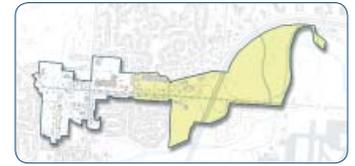
Architecture within the vehicular district will differ from that of the pedestrian district in several ways. Most notably, the scale of the vehicular district can be larger than that of the pedestrian, as buildings and sites will be accessed primarily through automobiles. Materials and details can likewise be adjusted to the scale of the automobile. The style of architecture can vary dependent upon its use, but all buildings in the district should be constructed of quality materials.

Stakeholders and open house participants had a number of opinions in regards to the acceptable scale and height of buildings within the district. To some extent, people seemed uncomfortable with tall buildings close to the roadway, but felt more comfortable when the building had a larger setback. The styles that were preferred were more traditional in nature, and all had straightforward massing and traditional detailing. People tended to dislike architecture that looked modern or was considered too large for the area.

### Design Intent Statement

***Buildings should be of a recognizable style and character to the district, constructed of high quality materials. The scale of the building should be appropriate to its surroundings.***

- Aim for a “timeless design” and employ sustainable /traditional materials and design that have a proven longevity.
- Buildings can be designed to the scale of the automobile, but should also provide visual interest to passers-by.
- Architectural elements such as doors, windows and columns should be used to create patterns, rhythm and depth along the facade of the building, providing visual interest to enrich the pedestrian experience.



### Likes



### Dislikes



## Vehicular District: *Landscaping*

### Overview

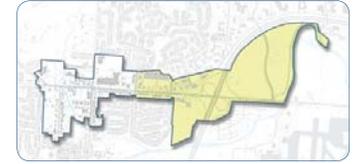
Landscaping is a critical component for a successful vehicular district. Vegetation and buffers should be used to soften the impact of street traffic and large areas of parking for both drivers and pedestrians.

Stakeholders and open house participants generally preferred landscaping that is dense, green, consistent and creates a strong edge. Landscaping that seemed irregular or disjointed was not typically preferred.

### Design Intent Statement

***Landscaping in the vehicular district should lessen the impacts of busy streets and large areas of parking. The landscaping should be low maintenance, create a distinct image to the district and enhance the streetscape and public realm.***

- Use massing to create arrangements of color and texture to soften the edges of streets, parking and buildings. Species should be grouped together to create the best impact.
- Create, continue and expand upon seasonal planting programs in a systematic and comprehensive way.
- Landscaping should be used to define the boundary between automobile, pedestrian and building zones, as well as provide buffering between streets, sidewalks, parking and buildings.
- Landscaping should be used to communicate the entrances of the site and the buildings.



### Likes



### Dislikes



## Vehicular District: Signage

### Overview

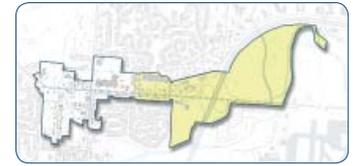
Signage within the vehicular district must balance the need to be visible from a passing car while maintaining an appropriate size, scale and quality for the district.

Open house participants and stakeholders tended to prefer signage that was smaller and offered more restraint in graphics and lighting. Materials were typically natural and tended to avoid back-lit plastic. People overwhelmingly disliked signage of inappropriate scale, citing the distracting or cluttered appearance of signage competing for dominance.

### Design Intent Statement

***Signage should convey a sense of cohesiveness and unity, while practicing restraint in size, color, uniqueness and illumination.***

- Signs should be conceived as an integral part of the architecture, landscape and lighting so as not to appear as an afterthought.
- The location, size and appearance of signs should complement the building and be complementary in character with the rest of the corridor.
- Landscaping shall be used to soften the base of ground signage.
- Exterior indirect lighting fixtures are the preferred source of illumination.
- Signage should have a clear hierarchy of information.



### Likes



### Dislikes



## Vehicular District: *Streetscape*

### Overview

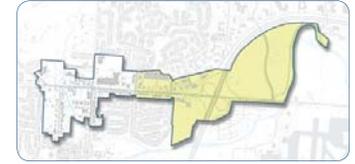
Properly designed streetscapes will reinforce the character and establish the identity of the corridor. Consistency in design, with quality materials carried throughout both the public and private areas are necessary to create an effective streetscape. Site elements pulled closer to the road will create a better sense of “place” as well as act to calm traffic.

Open house participants and stakeholders preferred streetscapes that were uncluttered and used high quality materials and fixtures. Streetscapes with continuous connectivity between adjacent uses was also preferred by the participants.

### Design Intent Statement

***Streetscape design should be consistent throughout the district, and use high quality materials in order to establish and reinforce the quality and character of the district.***

- Street trees and landscaping should be used to define the boundary between automobile and pedestrian zones, as well as provide buffering between the street, sidewalk and parking lots.
- Pedestrian connections should be provided throughout the district, with adequate protection between pedestrians and street. Pedestrian and vehicle conflicts should be minimized.
- Streetscape amenities and landscape features should create a consistent appearance with street trees, plantings and high quality furnishings.



### Likes



### Dislikes



## Vehicular District: *Lighting*

### Overview

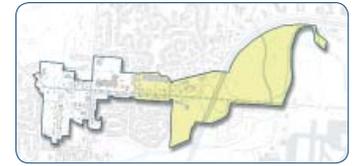
Lighting within the district should provide a safe and accessible atmosphere for both drivers and pedestrians. The design of lighting and light fixtures will help to establish a consistent design language and character throughout the district.

Stakeholders and open house participants tended to prefer lighting that was traditional and disliked lighting that was too modern or bold. Likewise, they tended to dislike “shoebox” or “cobrahead” type fixtures commonly seen in parking areas.

### Design Intent Statement

***Lighting should represent and reinforce the desired character of the district, and provide safety and accessibility for both drivers and pedestrians.***

- Lighting should promote pedestrian and vehicular safety and add to the character of the corridor.
- The style of the pole and fixture should promote the character of the corridor. Fixtures and poles should match the same size, style and color as those used in historic downtown Gahanna.



### Likes



### Dislikes



## Vehicular District: *Public Space*

### Overview

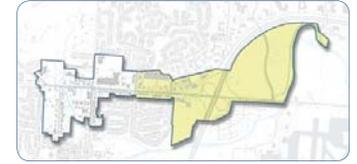
Public spaces designed to serve employees and visitors within work places will encourage and provide for pedestrian and leisure activities within the automotive district. Although automobiles will be the primary mode of transportation, public spaces can serve as place of respite for workers, visitors, shoppers and pedestrians to enjoy.

Stakeholders and open house participants tended to prefer spaces with a large amount of green vegetation that provided areas for seating and shade. Spaces lacking large amounts of vegetation and those that seemed visually cluttered were generally disliked.

### Design Intent Statement

***Public spaces should be designed to serve employees and visitors within work places. These spaces should be used to attract employees and businesses to the area. These spaces can be both publically or privately owned, and can include spaces such as plazas, courtyards, cafe spaces and green spaces.***

- Provide public spaces in high use areas.
- Consider function, size and activity when designing public spaces.
- Provide an adequate amount of seating and pedestrian amenities in public spaces.
- Provide public spaces in high use areas.



### Likes



### Dislikes



## Vehicular District:

### Site Design

#### Overview

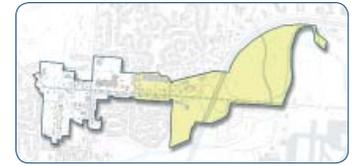
Site design within the vehicular district will primarily involve placement of the building and parking in relation to each other and the roadway. Generally, an attempt should be made to lessen the intensity of parking within the district. This can be done by splitting up lots into smaller sizes, placing parking to the side or rear of the building, and incorporating landscaping. Building placement should also generally correlate to its setback from the road, with larger buildings having a larger setback.

Stakeholders and open house participants tended to prefer site designs that concealed parking from the view of the street, and had some amount of greenspace buffer in the front. People tended to dislike retail shopping centers that placed large areas of parking in front of single-story buildings.

#### Design Intent Statement

***Site design within the vehicular district should attempt to bring the building closer to the road and minimize the impact and view of parking lots. Building setbacks should be based upon the overall scale and height of the building.***

- Provide setbacks appropriate to the district.
- Building should be close enough to engage the street but setback far enough to allow for a buffer zone and pedestrian zone between building and street.
- Parking lots, loading, utilities and waste containers should be located to the rear or side of the building. When located to the side, landscape screening should be used to shield their visibility from the street.
- Large expanses of parking should be avoided or broken up with landscaping or other features.



#### Likes



#### Dislikes



## Plan Recommendations



# 4a

## Overview

The recommendations within this section are intended to be specific actions the City of Gahanna can take to encourage the character and development described within this Plan. The goal was to develop a set of recommendations for the Plan that both the City and private individuals can use as a guide to direct future development within the corridor.

## Create an Overlay District

The establishment of an Overlay District will be a necessary step in implementing the vision outlined within the Plan. The Overlay District should contain design guidelines or standards that reflect the overall vision of the corridor. This will make it easier to build quality projects that conform to the desired character of the corridor. The review and enforcement of the Overlay can be handled by an existing review body such as the Planning Commission. Recommendations contained within the Overlay should be clear, concise and easily understood, with some amount of flexibility to account for the particular circumstances of each project.

## Promote the City's Redevelopment Efforts

A special effort should be taken to inform residents as well as existing and prospective businesses of the redevelopment efforts that the City is making within the Corridor. Information should be clear, concise and convey the extent and character of the City's planned or completed improvements.

## Coordinate Branding Efforts

The Hamilton Road Corridor should employ a cohesive and coherent branding strategy that directs the character of both public improvements and private development. Branding within the corridor should be based on the existing branding strategy in Gahanna, but identify the corridor as a different district within the City.

## Streetscape Improvements

To succeed in redeveloping the corridor, the City must show visible results that can come from completing projects. Frequent, visible changes are a way to show the revitalization effort is taking shape, and streetscape improvements are an effective way to do this. These improvements should be part of the overall branding strategy of the corridor and act as the City's primary investment in the overall promotion and

redevelopment of the district.

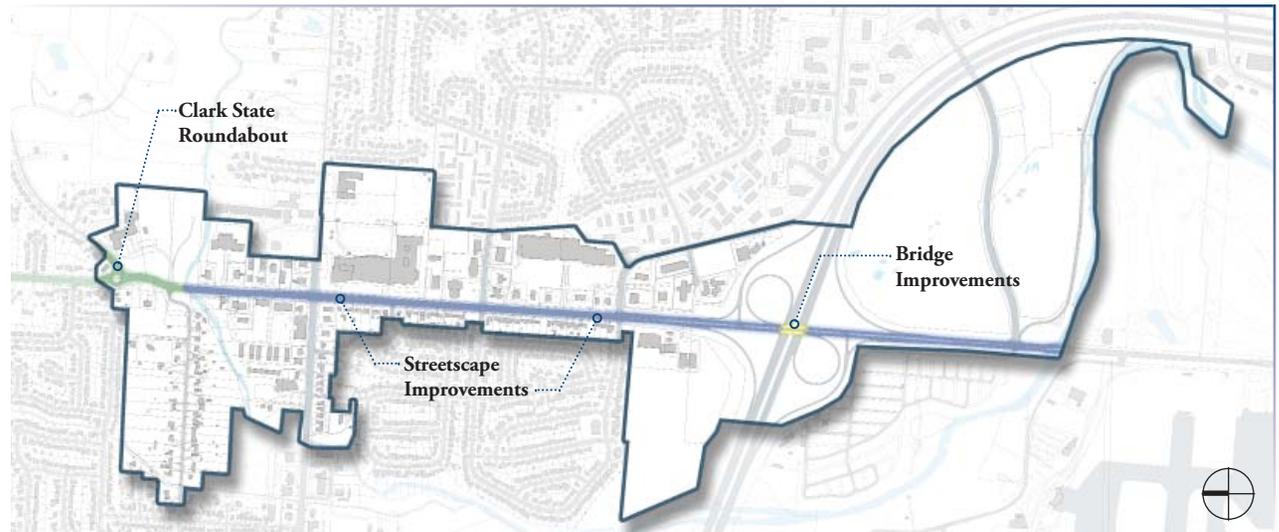
## Bridge Improvements

The large amount of traffic passing under the I-270 freeway overpass is a great opportunity to reinforce the branding and redevelopment efforts taking place within the corridor. The bridge will act as one of the primary gateways into the corridor area and Gahanna as a whole.

## Roundabout Improvements

The roundabout to the north end of the corridor will act as a primary gateway for people traveling south on Hamilton Road. The character of the roundabout should signify a transition from the auto-centric streets into the pedestrian-oriented district. The City should use every opportunity to beautify and enhance this roundabout, as it will be a main entry into the district.

Map 8: Recommended Improvements

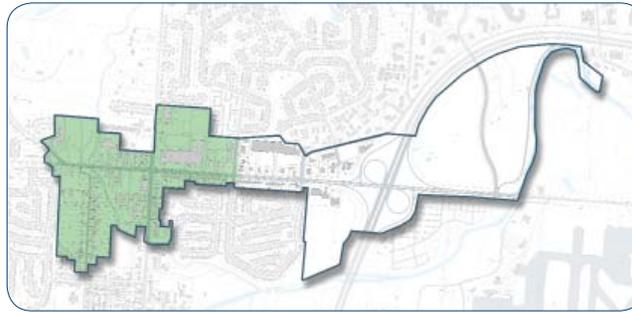


## Development Recommendations



# 4b

**Development  
Recommendations:**  
*Pedestrian District*



<p><b><i>Architecture - Pedestrian District</i></b></p> <p>Architecture should be high-quality and include a diversity of architectural styles to encourage visual interest and add to the overall appeal of the corridor. Building massing, shape, natural materials and traditional details should all complement pedestrian activity.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Aim for a “timeless design” and employ sustainable/traditional materials and design that have a proven longevity.</li> <li><input type="checkbox"/> The building facade should be varied to provide visual interest, while contributing to the quality and definition of the street wall.</li> <li><input type="checkbox"/> Buildings should be designed to the scale of the pedestrian, utilizing traditional detailing and architectural elements such as doors, windows and columns to create patterns and rhythm along the building facade.</li> </ul>
<p><b><i>Landscaping - Pedestrian District</i></b></p> <p>Landscaping should be low maintenance, create a distinct image to the corridor, integrated to the design of the site, and enhance the streetscape and public realm.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Enhance the street and pedestrian environment through careful selection and placement of street trees and planting beds.</li> <li><input type="checkbox"/> Use massing to create arrangements of color and texture to soften the built environment; where applicable, species should be grouped together to create the best impact.</li> <li><input type="checkbox"/> Seasonal planting programs should be performed in a systematic and comprehensive way.</li> <li><input type="checkbox"/> Landscaping should be used to define the boundary between automobile, pedestrian and building zones, as well as provide buffering between the sidewalk and street.</li> </ul>
<p><b><i>Signage - Pedestrian District</i></b></p> <p>Signage should be uniform, eclectic, and unique, promoting the quality and character of the corridor.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Signs should be conceived as an integral part of the architecture, landscape and site design so as not to appear as an afterthought. The location, size and appearance of signs should complement the building and be complementary in character with the rest of the corridor.</li> <li><input type="checkbox"/> Landscaping should be used to soften the base of ground signage.</li> <li><input type="checkbox"/> Exterior indirect lighting fixtures are the preferred source of illumination.</li> <li><input type="checkbox"/> Temporary sandwich board signs that add to the character of the pedestrian zone and streetscape, should be allowed.</li> </ul>

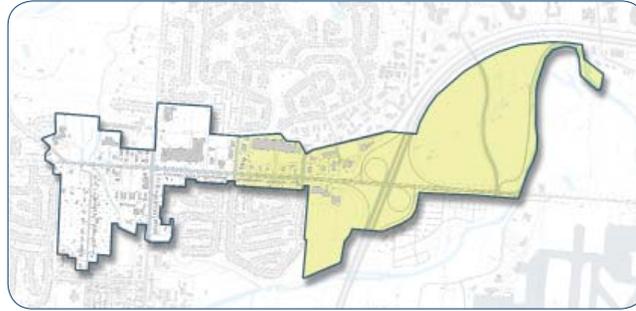


# Development Recommendations: Pedestrian District (cont.)

<p><b>Streetscape - Pedestrian District</b></p> <p>Streetscape improvements should convey a clean, simple, and consistent image that is defined by signature street trees, a unified plant palette, and identifiable landscape elements in targeted areas.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Streetscapes should be walkable and accommodate a variety of uses.</li> <li><input type="checkbox"/> Site furnishings should be provided to enliven the streetscape and provide areas for rest and interaction.</li> <li><input type="checkbox"/> Building setbacks should be wide enough to create a pedestrian zone 10'-20' from the edge of the roadway to promote pedestrian activity as well as dining and other commercial activities.</li> <li><input type="checkbox"/> Street trees and landscape should be used to establish a cohesive streetscape.</li> </ul>
<p><b>Lighting - Pedestrian District</b></p> <p>Pedestrian lights should contribute to and reinforce the character of the district and the pedestrian scale of the street. Fixtures should be as consistent in size, style and color as possible throughout the corridor for both public and private developments.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Lighting should promote pedestrian and vehicular safety and add to the character of the corridor.</li> <li><input type="checkbox"/> The style of the pole and fixture should promote the character of the corridor. Fixtures and poles should match the same size, style and color as those used in historic downtown Gahanna.</li> <li><input type="checkbox"/> Pedestrian lights should contribute to the pedestrian scale of the street and add a warm glow of light on the sidewalk.</li> </ul>
<p><b>Public Space - Pedestrian District</b></p> <p>Public spaces should be open to all, well designed and promote various uses and activities. Plazas, courtyards, cafe spaces, green spaces and pocket parks are examples of public spaces that can attract people to the area and promote pedestrian activity. These spaces can be both publically or privately owned.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Provide public spaces in high use areas.</li> <li><input type="checkbox"/> Consider function, size and activity when designing a public space.</li> <li><input type="checkbox"/> Provide an adequate amount of seating and pedestrian amenities in public spaces.</li> <li><input type="checkbox"/> Integrate the use of plant material and features to enliven public spaces and promote safety.</li> </ul>
<p><b>Site Design - Pedestrian District</b></p> <p>Site elements that enhance pedestrian activity and create a feeling of place, should be located closer to the street to promote and enhance pedestrian activity within the district.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Building setbacks should be wide enough to create a pedestrian zone 10'-20' in width from the curb to promote pedestrian activity as well as dining and other commercial activities.</li> <li><input type="checkbox"/> The primary building entrance and storefront windows shall face the street.</li> <li><input type="checkbox"/> Parking lots, loading, utilities and waste containers shall be located to the rear or side of the building. When located to the side, landscape screening shall be used to shield their visibility from the street.</li> </ul>



**Development  
Recommendations:**  
*Vehicular District*



<p><b><i>Architecture - Vehicular District</i></b></p> <p>Buildings should be of a recognizable style and character to the district, constructed of high quality materials. The scale of the building should be appropriate to its surroundings.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Aim for a “timeless design” and employ sustainable /traditional materials and design that have a proven longevity.</li> <li><input type="checkbox"/> Buildings can be designed to the scale of the automobile, but should also provide visual interest to passers-by.</li> <li><input type="checkbox"/> Architectural elements such as doors, windows and columns should be used to create patterns, rhythm and depth along the facade of the building, providing visual interest to enrich the pedestrian experience.</li> </ul>
<p><b><i>Landscaping - Vehicular District</i></b></p> <p>Landscaping in the vehicular district should lessen the impacts of busy streets and large areas of parking. The landscaping should be low maintenance, create a distinct image to the district and enhance the streetscape and public realm.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Use massing to create arrangements of color and texture to soften the edges of streets, parking and buildings. Species should be grouped together to create the best impact.</li> <li><input type="checkbox"/> Create, continue and expand upon seasonal planting programs in a systematic and comprehensive way.</li> <li><input type="checkbox"/> Landscaping should be used to define the boundary between automobile, pedestrian and building zones, as well as provide buffering between streets, sidewalks, parking and buildings.</li> <li><input type="checkbox"/> Landscaping should be used to communicate the entrances of the site and the buildings.</li> </ul>
<p><b><i>Signage - Vehicular District</i></b></p> <p>Signage should convey a sense of cohesiveness and unity, while practicing restraint in size, color, uniqueness and illumination.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Signs should be conceived as an integral part of the architecture, landscape and lighting so as not to appear as an afterthought.</li> <li><input type="checkbox"/> The location, size and appearance of signs should complement the building and be complementary in character with the rest of the corridor.</li> <li><input type="checkbox"/> Landscaping shall be used to soften the base of ground signage.</li> <li><input type="checkbox"/> Exterior indirect lighting fixtures are the preferred source of illumination.</li> <li><input type="checkbox"/> Signage should have a clear hierarchy of information.</li> </ul>



# Development Recommendations: *Vehicular District* (cont.)

<p><b><i>Streetscape - Vehicular District</i></b></p> <p>Streetscape design should be consistent throughout the district, and use with high quality materials in order to establish and reinforce the quality and character of the district.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Street trees and landscaping should be used to define the boundary between automobile and pedestrian zones, as well as provide buffering between the street, sidewalk and parking lots.</li> <li><input type="checkbox"/> Pedestrian connections should be provided throughout the district, with adequate protection between pedestrians and street. Pedestrian and vehicle conflicts should be minimized.</li> <li><input type="checkbox"/> Streetscape amenities and landscape features should create a consistent appearance with street trees, plantings and high quality furnishings.</li> </ul>
<p><b><i>Lighting - Vehicular District</i></b></p> <p>Lighting should represent and reinforce the desired character of the district, and provide safety and accessibility for both drivers and pedestrians.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Lighting should promote pedestrian and vehicular safety and add to the character of the corridor.</li> <li><input type="checkbox"/> The style of the pole and fixture should promote the character of the corridor. Fixtures and poles should match the same size, style and color as those used in historic downtown Gahanna.</li> </ul>
<p><b><i>Public Space - Vehicular District</i></b></p> <p>Public spaces should be designed to serve employees and visitors within work places. These spaces should be used to attract employees and businesses to the area. These spaces can be both publically or privately owned, and can include spaces such as plazas, courtyards, cafe spaces and green spaces.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Provide public spaces in high use areas.</li> <li><input type="checkbox"/> Consider function, size and activity when designing a public space.</li> <li><input type="checkbox"/> Provide an adequate amount of seating and pedestrian amenities in public spaces.</li> </ul>
<p><b><i>Site Design - Vehicular District</i></b></p> <p>Site design within the vehicular district should attempt to bring the building closer to the road and minimize the impact and view of parking lots. Building setbacks should be based upon the overall scale and height of the building.</p>	<ul style="list-style-type: none"> <li><input type="checkbox"/> Provide setbacks appropriate to the district.</li> <li><input type="checkbox"/> Building should be close enough to engage the street but setback far enough to allow for a buffer zone and pedestrian zone between building and street.</li> <li><input type="checkbox"/> Parking lots, loading, utilities and waste containers should be located to the rear or side of the building. When located to the side, landscape screening should be used to shield their visibility from the street.</li> <li><input type="checkbox"/> Large expanses of parking should be avoided or broken up with landscaping or other features.</li> </ul>



